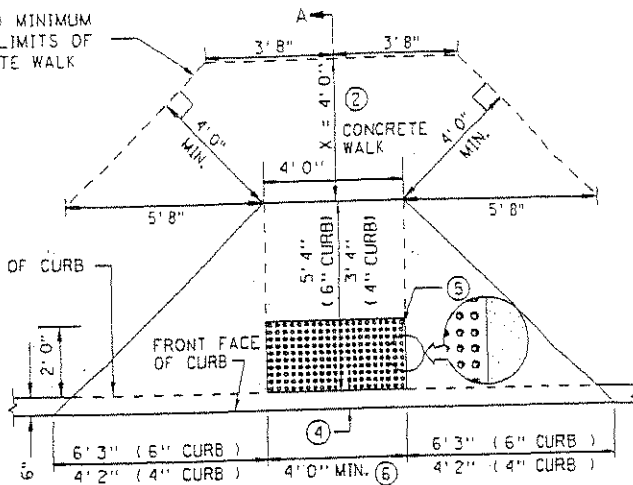
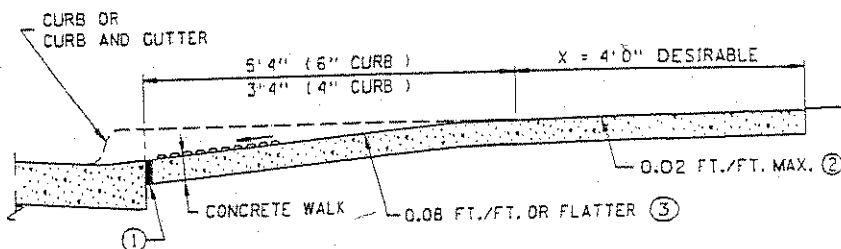


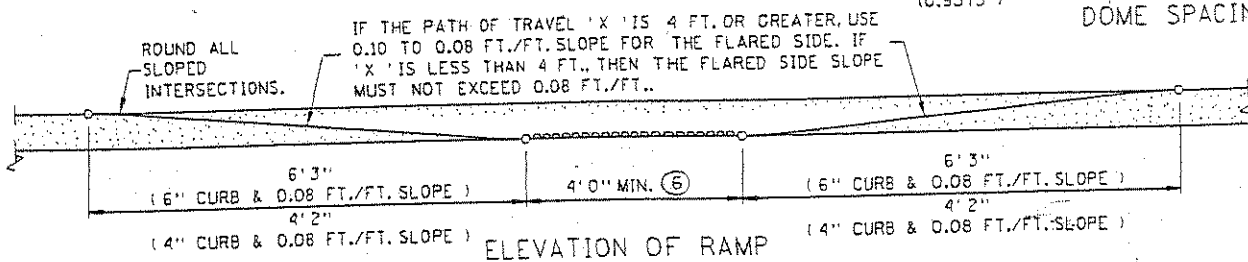
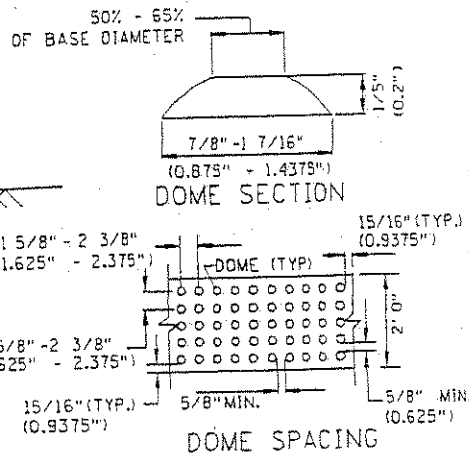
PLAN VIEW OF DIAGONAL RAMP



PLAN VIEW OF PERPENDICULAR RAMP



SECTION A-A



ELEVATION OF RAMP

NOTES:

TO COMPLY WITH THE AMERICANS WITH DISABILITIES ACT (ADA), ALL STATE AND LOCAL AGENCIES ARE REQUIRED TO COMPLY WITH THIS STANDARD PLATE.

THE CURB AND CURB TRANSITION ON THE RAMP WILL BE PAID FOR AS LINEAR FEET OF CONCRETE CURB OR CONCRETE CURB AND GUTTER. THE RAMP AREA WILL BE PAID FOR AS CONCRETE WALK EXCLUDING THE TRUNCATED DOME AREA WHICH WILL BE PAID FOR AS TRUNCATED DOMES BY THE SQUARE FOOT. BIKE TRAILS SHALL HAVE TRUNCATED DOMES ACROSS THE ENTIRE WIDTH OF TRAIL WHEN THE TRAIL CROSSES A ROAD. DOMES ARE NOT TO BE USED ON SIDEWALKS OR TRAILS WHEN CROSSING ALLEYS OR DRIVEWAYS.

- ① 1/2 INCH PREFORMED JOINT FILLER MATERIAL, AASHTO M 213.
- ② WHEN POSSIBLE, PROVIDE A CLEAR PATH OF TRAVEL 4'0" WIDE BEHIND THE PEDESTRIAN RAMP, A RELATIVELY FLAT 4' X 4' LANDING WILL ALLOW WHEELCHAIRS TO NAVIGATE AROUND THE PEDESTRIAN RAMP, NO SIGNALS, SIGNS, CABINETS, OR OTHER OBSTRUCTIONS ARE ALLOWED IN THE RAMP OR PATH OF TRAVEL.
- ③ WHEN A MEDIAN IS NOT WIDE ENOUGH FOR TWO PEDESTRIAN RAMPS AND A 48" LANDING BETWEEN THEM, THE PEDESTRIAN CROSSING SHALL BE CUT THROUGH THE MEDIAN AT STREET LEVEL.
- ④ PLACE THE DETECTABLE WARNINGS (TRUNCATED DOMES) AT THE BACK OF CURB. WHEN THE DETECTABLE WARNING SYSTEM IS A PRECAST MATERIAL, THE CURB SHALL BE HAND FORMED TO FILL THE GAP.
- ⑤ ADA REQUIRED TRUNCATED DOME AREA SHALL BE 2'0" MIN. IN DIRECTION OF TRAVEL AND SHALL EXTEND THE FULL WIDTH (3'0" OR 4'0" TYP.) OF THE CURB RAMP. THIS 2'0" BY 3'0" OR 4'0" WIDTH (TYP.) TRUNCATED DOME AREA SHALL CONTRAST VISUALLY WITH THE ADJACENT WALKING SURFACE. THE ENTIRE TRUNCATED DOME AREA SHALL BE A LIGHT (GRAY OR BUFF TYPICALLY) COLOR WHEN THE ADJACENT SIDEWALK IS A DARK COLOR. THE ENTIRE TRUNCATED DOME AREA SHALL BE A DARK COLOR (RED OR DARK GRAY TYPICALLY) WHEN THE ADJACENT SIDEWALK IS A LIGHT GRAY CEMENT COLOR.
- ⑥ 4'0" MIN. FOR NEW CONSTRUCTION. 3'0" ALLOWED TO STAY IN PLACE FOR RETROFIT PROJECTS. IN SITUATIONS WHERE THE CURB CUT WIDTH EXCEEDS 4'0", THE DETECTABLE WARNINGS SHALL EXTEND THE FULL WIDTH.

APPROVED FEB. 20, 2004

M. Rakus
STATE DESIGN ENGINEER

STATE OF MINNESOTA
DEPARTMENT OF TRANSPORTATION

PEDESTRIAN CURB RAMP
FOR THE HANDICAPPED

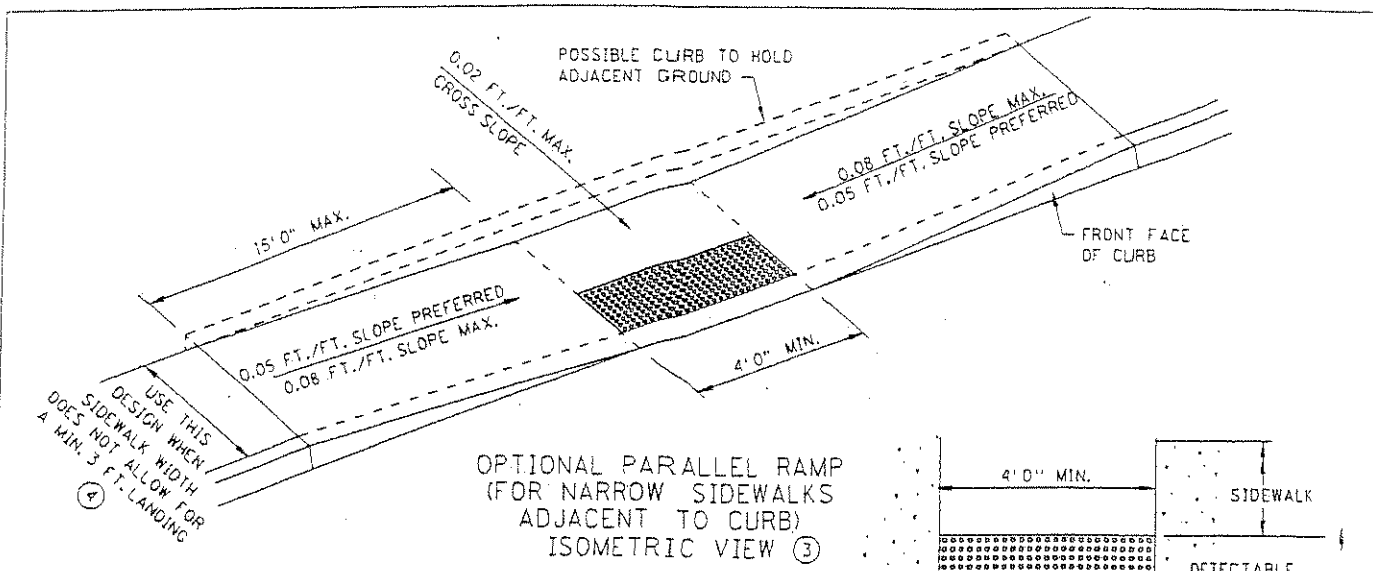
SPECIFICATION
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2521
2531

REVISION DATE
4-14-2004

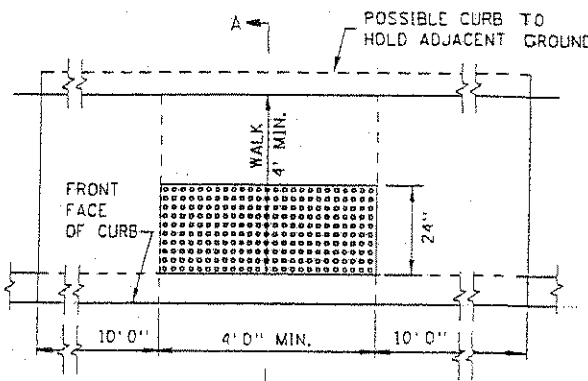
STANDARD
PLATE
NO.

7036F

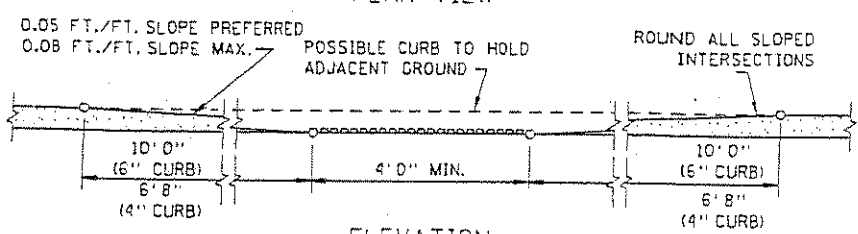
1 OF 2



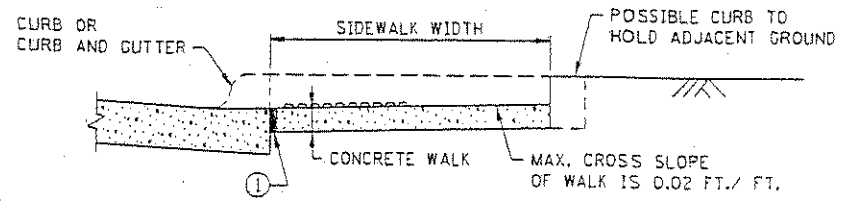
OPTIONAL PARALLEL RAMP
(FOR NARROW SIDEWALKS
ADJACENT TO CURB)
ISOMETRIC VIEW ③



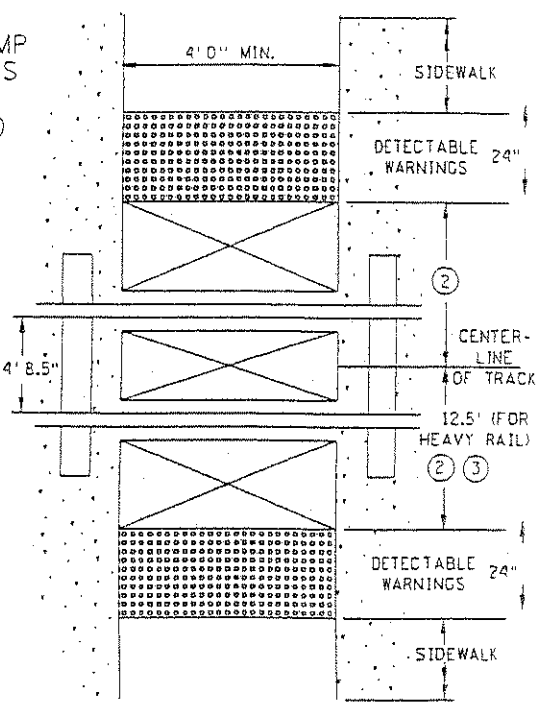
PARALLEL RAMP
PLAN VIEW



ELEVATION



PARALLEL RAMP
SECTION A-A



RAILROAD CROSSING
PLAN VIEW

NOTES:

- ① 1/2 INCH PREFORMED JOINT FILLER MATERIAL, AASHTO M 213.
- ② WHEN THERE IS A GATE ARM, THE EDGE OF THE DETECTABLE WARNING SHALL BE 17"-19" FROM THE FACE OF THE GATE ARM. WHEN THERE IS NO GATE, THE EDGE OF THE DETECTABLE WARNING SHALL BE 12.5 FT. (HEAVY RAIL) OR 8 FT. (LRT) FROM THE CENTERLINE OF THE NEAREST TRACK.
- ③ ALL RAIL IN STATE IS HEAVY RAIL WITH THE EXCEPTION OF THE LRT IN MINNEAPOLIS.
- ④ THE REASON FOR USING THIS VARIATION INSTEAD OF THE OFFICIAL DESIGN ON SH. 1 SHALL BE DOCUMENTED. IF A LARGE AMOUNT OF WATER IS EXPECTED FROM THE ADJACENT PROPERTY, THE DESIGNER SHOULD PLAN ACCORDINGLY.

THESE ARE OPTIONAL PEDESTRIAN SIDEWALK ACCESS DETAILS THAT ARE NOT REQUIRED BY CURRENT ADA REGULATIONS

APPROVED FEB. 20, 2004
M. Rakus
STATE DESIGN ENGINEER

STATE OF MINNESOTA
DEPARTMENT OF TRANSPORTATION
PEDESTRIAN CURB RAMP OPTIONS
FOR THE HANDICAPPED

SPECIFICATION REFERENCE	STANDARD PLATE NO.
2521 2531	7036F 2 OF 2